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# Framingham Bicycle & Pedestrian Plan 2016

**Update Workshop – May 4, 2016**



An aerial photograph showing a large, circular, light-colored pattern on a dark surface. The pattern consists of several concentric, slightly irregular rings, creating a target-like appearance. The text "What is the status of the Plan?" is overlaid in the center of the image.

**What is the status of the Plan?**

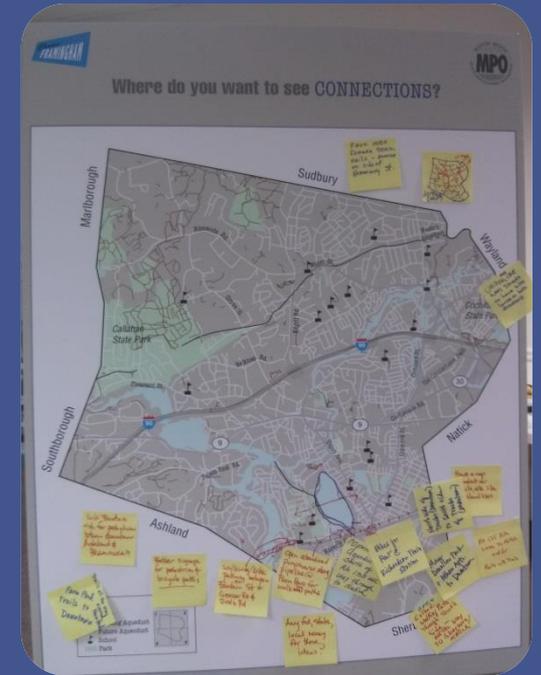
# Phase 1: Public Engagement Establishment of Goals & Objectives

Livable  
Community  
Workshop (1<sup>st</sup>  
public charrette)  
– Sept 2014

Internal Staff  
Charrette –  
February 2015

Goals & Objectives

List of roadways needing  
bike/pedestrian attention



# GOALS & OBJECTIVES

- 1. Identify the Assets**
- 2. Connect Assets into a Usable Bicycle & Pedestrian Network**
  - Prioritize projects for bike & pedestrian implementation (evaluation matrix and mapping)
- 3. Maintain the Bicycle & Pedestrian Network**
  - Implement public reporting system of maintenance issues
  - Framingham Department of Public Works walk roads once a year to identify problems for bike & pedestrian and note for plan inclusion
- 4. Design for Bicycle & Pedestrian Safety**
  - Encourage and increase Safe Routes to School programming across Framingham schools
- 5. Communicate Bicycle & Pedestrian Routes through Education and Signage**
  - Create map handout/info packet for trails and/or bike routes. Investigate feasibility of smartphone app.
  - Rules of the Road education in schools
- 6. Provide Seamless Links to Transit**
  - Bring a rental bike program, such as Hubway, to Framingham
  - Expand MetroWest Regional Transit Authority bus service
- 7. Include Bicycle & Pedestrian Access in Land Use Planning**
  - Encourage parking behind buildings in order to retain a high quality bike & pedestrian environment along the roadways
  - Map existing bicycle racks across Town and increase inventory in key areas

With the **Goals & Objectives**.....

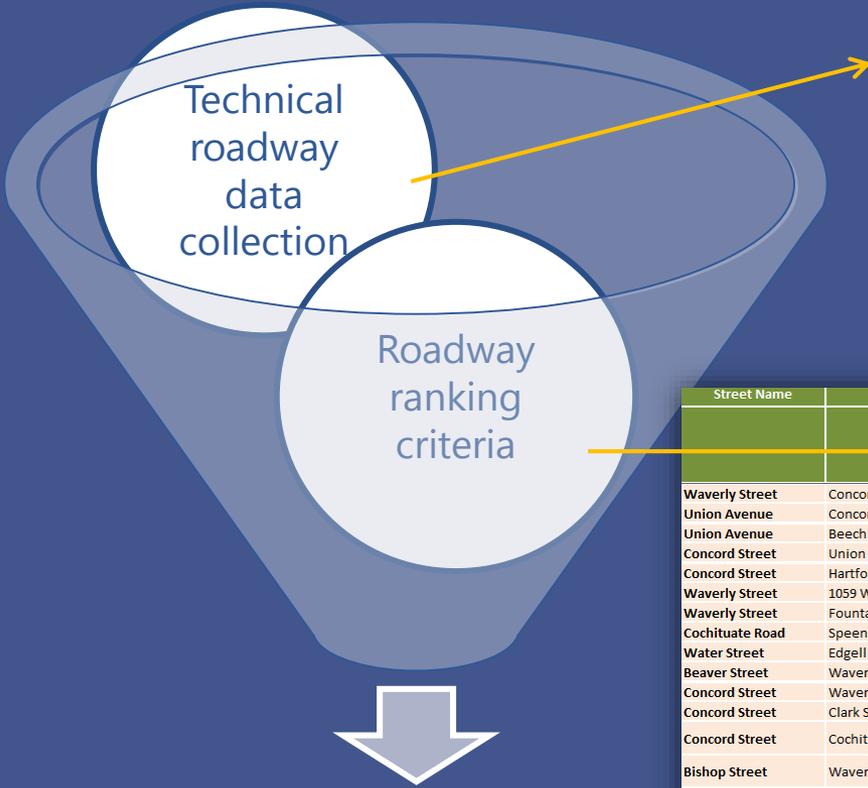
And the **list of roadways** that the public & staff felt needed bike & pedestrian attention.....

We moved onto Phase 2



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# Phase 2: Roadway Prioritization Plan Development



These include.....

- Roadway width
- Number of lanes
- Direction of travel
- Presence & condition of sidewalks
- Handicapped accessibility
- Presence of bike signage & bike lanes
- Etc.

Street Name	From	To	Prioritization Criteria (see "Prioritization Key" tab for score instructions)									Total Score (of 100)
			# of EJ Criteria Met (0-3)	Population density (0-3)	Accidents in Last 5 yrs. (0-3)	Destinations within 1/4 mi. (0-3)	MAPC Utility Score (1-3)	Cost Relative to Benefit (1-3)	DPW Planned Project (0-3)	Bike Conditions (0-3)	Ped. Conditions (0-3)	
Waverly Street	Concord St.	Natick Townline	3	3	3	3	3	3	2	3	2	92
Union Avenue	Concord St.	Beech St.	2	3	2	3	3	3	1	3	2	83
Union Avenue	Beech St.	Maple St.	2	3	2	3	3	3	1	3	2	83
Concord Street	Union Ave.	Clark St.	3	3	2	3	3	3	0	3	1	78
Concord Street	Hartford St.	Valentine Rd.	2	3	3	3	3	3	0	3	1	78
Waverly Street	1059 Waverly St.	Fountain St.	2	2	2	3	3	2	2	3	2	78
Waverly Street	Fountain St.	Concord St.	1	3	3	3	3	3	2	3	1	78
Cochituate Road	Speen St.	Shoppers World	2	1	2	2	3	1	2	3	3	75
Water Street	Edgell Rd.	Central St.	0	3	2	3	3	3	0	3	2	75
Beaver Street	Waverly Street	Blandin Ave	3	3	0	1	3	3	1	3	2	73
Concord Street	Waverly St.	Union Ave.	3	3	3	3	3	2	0	2	1	72
Concord Street	Clark St.	Hartford St.	2	3	3	3	3	3	0	2	1	72
Concord Street	Cochituate Rd.	School St.	1	2	1	3	3	2	2	3	2	72
Bishop Street	Waverly	Arthur	2	3	1	1	3	2	2	3	2	72
Concord Street	Valentine Rd.	Cochituate Rd.	1	3	1	2	3	3	0	3	2	72
Mt. Wayte Avenue	Union Ave.	Franklin St	2	3	1	1	3	2	2	3	2	72
Western Avenue	Herring Dr.	Sherborn	3	2	1	1	3	3	3	2	2	70
Dudley Road	Mount Wayte Ave	Fountain St.	2	1	1	2	3	2	2	3	2	68
Mt. Wayte Avenue	Franklin St	Dudley St.	2	3	1	1	3	3	2	3	1	68

Roadway Prioritization  
Matrix



Phase 2 provided the list of roadways needing bike & pedestrian attention in a **prioritized order**.....

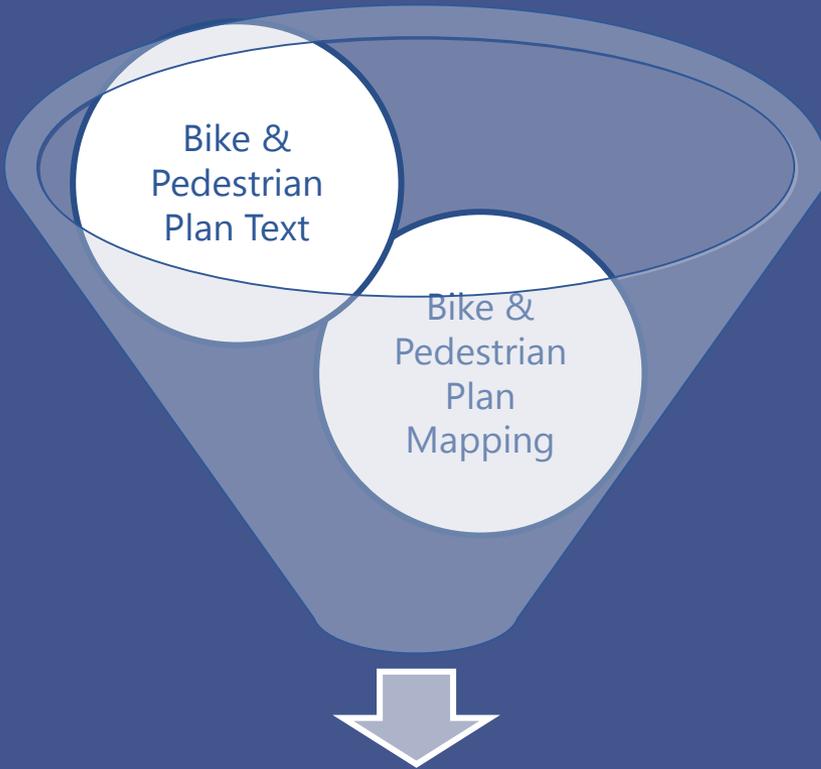
Which establishes a plan moving forward

Phase 3 – active now – involves **finalizing the Plan's** text/mapping & submitting an application under the **Complete Streets Program** to kickstart implementation



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**Phase 3:**  
**Finalize Plan**  
**Complete Streets Program Submittal**



Draft Framingham Bike & Pedestrian Plan 2016

Timeline moving forward:	
Public Open House Plan Update	May 4, 2016
Meetings with applicable boards	May 2016
Submit application under MassDOT's Complete Streets Program	July-August 2016
Finalize Framingham Bike & Pedestrian Plan 2016 (text & mapping)	May-August 2016

You are here

More on this program on the next few slides!

# What is a Complete Street?

A roadway providing safe and accessible options for **all** travel modes – **walking**, **biking**, **transit**, and **vehicles** – for people of **all** ages and abilities.



Imagine walking or biking along this street.....



Versus walking or biking along this street....



**Wide sidewalks**

**Protected bike lane**

**Painted crosswalks**

This is a Complete Street.....



**Handicapped accessible ramp**

**Dedicated median for pedestrians**

**No bike lanes – very wide roadway can accommodate bike lanes and sidewalks**

This is far from a Complete Street.....



**No sidewalks – note this desire line where pedestrians are walking**

**Where are the crosswalks?  
Where do children and elderly walk?**

# Framingham adopted a Complete Streets policy in January 2015 committing to incorporating Complete Streets elements in all roadway projects

*Smart Growth America scored Framingham's policy #9 in the country out of policies passed in 2015.*

*Mass Department of Transportation scored Framingham's policy an 88 out of 100 points.*



Town of Framingham  
Board of Selectmen

## Policy on Complete Streets

Issue date: January 6, 2015  
Type of policy: New (x) Amendment ( )  
Effective date: January 6, 2015

Level: Department ( ) Division ( ) Town Wide (x)

### Policy Statement

Complete Streets principles will contribute toward the safety, health, economic vitality, and quality of life in the Town of Framingham by providing accessible and efficient connections between residential, educational, commercial, recreational, civic, and retail destinations by improving multi-modal environments throughout the Town's urban, suburban, and rural neighborhoods. Complete Streets are designed and operated to provide safety and accessibility for all users of Framingham's roadways, trails, and transit systems, including pedestrian, bicyclists, transit riders, motorists, commercial vehicles, and emergency vehicles and for people of all ages and of all abilities. The use of Complete Streets has been shown to have a positive impact on public health concerns, including improvements in air quality, promotion of physical activity, and enhanced access to healthier food options.

The purpose of Framingham's Complete Streets Policy is to enhance existing, create new, and strengthen connections between all transportation modes to accommodate all users through implementation of physical elements. The Town of Framingham will formalize the plan, design, operation, and maintenance of streets so that they are safe for all users of all ages and abilities. This Policy shall direct decision-makers to consistently plan, design, and construct streets to accommodate all anticipated users including, but not limited to pedestrians, bicyclists, motorists, emergency vehicles, and freight and commercial vehicles. In short, all transportation and development projects shall incorporate a Complete Streets philosophy that expands transportation choices for all users.

### References

1. Massachusetts Department of Transportation (MassDOT) Project Development & Design Guide (latest edition)
2. National Association of City Transportation Officials Urban Bikeway Design Guide (latest edition)
3. National Association of City Transportation Officials Urban Street Guide (latest edition)

With a **Complete Streets Policy....**

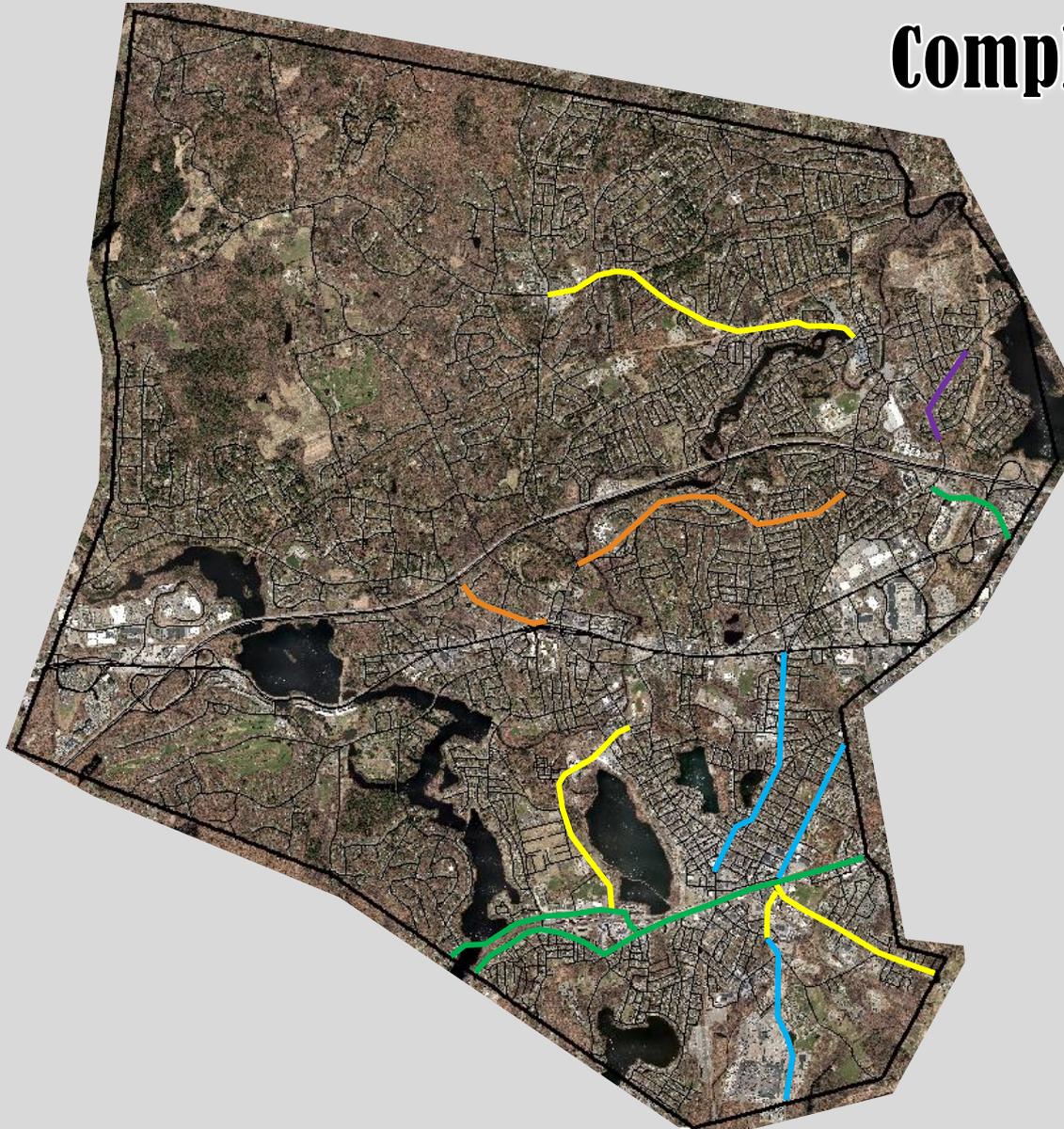
And a **roadway prioritization list....**

The Town can apply through MassDOT's **Complete Streets Program** for \$\$ towards bike & pedestrian implementation



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# Potential Projects for Complete Streets submittal



- Year 1
  - Water St
  - Mt Wayte Ave/Dudley Rd
  - Blandin Ave
  - Beaver St
- Year 2
  - Speen St
  - Fountain St
  - Waverly St
- Year 3
  - Concord Rd
  - Bishop St
  - Western Ave
- Year 4
  - Old Connecticut Path
- Year 5
  - Central St
  - Pleasant St

# A recap on the schedule:

<b>Public Open House Plan Update</b>	<b>May 4, 2016</b>
<b>Meetings with Traffic Roadway Safety Committee &amp; Board of Selectmen</b>	<b>May 2016</b>
<b>Submit application under MassDOT's Complete Streets Program</b>	<b>July-August 2016</b>
<b>Finalize Framingham Bike &amp; Pedestrian Plan 2016 (text &amp; mapping)</b>	<b>May-August 2016</b>

Once finalized, the Town will have its first ever Bike & Pedestrian Plan. The Plan will promote biking & walking projects through several Departments' day to day work such as Public Works. The Plan also places the Town in a better position to apply for competitive funding such as Complete Streets.



**Thank you for joining us! Please explore  
the stations around the room & ask us any  
questions.**

**Contact the Bike & Ped Working Group:**

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