

Downtown Framingham and Transit-Oriented Development

Roundtable Number 2

November 6th, 2013

Welcome & Introductions

Robert Halpin
Town Manager

Welcome

- Our panelists

- Roy S. MacDowell, Partner, Baystone Development Co.
- Ed Zuker, Principal, Chestnut Hill Realty
- Justin Krebs, Normandy Realty Partners

- Our organizing team

- Gene Kennedy, Interim Director, Community and Economic Development Division
- Erika Jerram, Senior Planner, Community and Economic Development Division
- Sarah Lewis, Architect/Urban Planner, Fuss & O'Neill
- David Foss, Hydrogeologist/LSP, Fuss & O'Neill

- And you, our attendees

March 5, 2013 Roundtable

Robert Halpin
Town Manager

Previous Discussion

“Why hasn’t Transit Oriented Development materialized in Downtown Framingham?”

- The need for density to create critical mass and justify aggregation of parcels
- The need for less restrictive off street parking reflecting the transit-oriented development market
- The need for flexibility and ability to meet changes in the market

Overview

Gene Kennedy

Interim Director, Community and
Economic Development Division

Brownfields Program

- Industrial Legacy
- 2008/09 first US EPA Brownfields Assessment Grants:
- 16 sites/114 acres total assessed to date
- Achievements:
 - Library Site, Pratt St, CRT
 - Danforth Green Reuse
- 2012-2015 Assessment Grant
- Area Wide Planning (AWP)

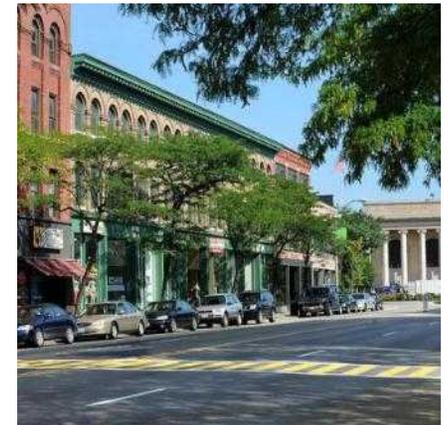
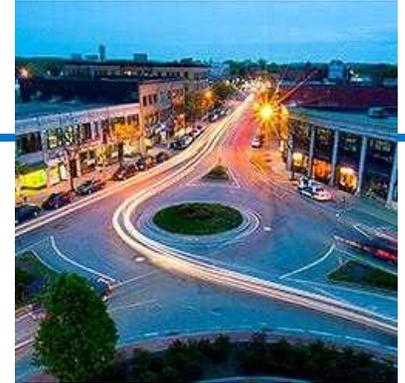


PERSPECTIVE FROM WATER STREET



Downtown Assets

- Location – Heart of MetroWest
- Access to student population
- Access to Transportation/Transit
- Proximity to jobs and training
- History and Architecture
- Cultural Amenities
- Open Space & Recreation



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Recent Private Investment: Arcade

- Arcade Project

- Permitted in 2004 originally for 290 units, 50,000 gsf of retail and a six level garage with 585 spaces.

- Current owner has invested heavily in the deferred maintenance and new residential units along the periphery

- Redeveloped 5 multi-family properties on Frederick St- increasing from 21 to 30 units total
 - Added 24 units in the Amsden Building on Kendall St.

Recent Private Investment: Other

- Kendall Building Façade improvements
- At least 3 new restaurants in the last year with 3 pending including Depot Diner and Subway.
- First Year a new Farmer's Market opened on the Downtown Common.
- ULI – Technical Assistance Panel for Howard Street

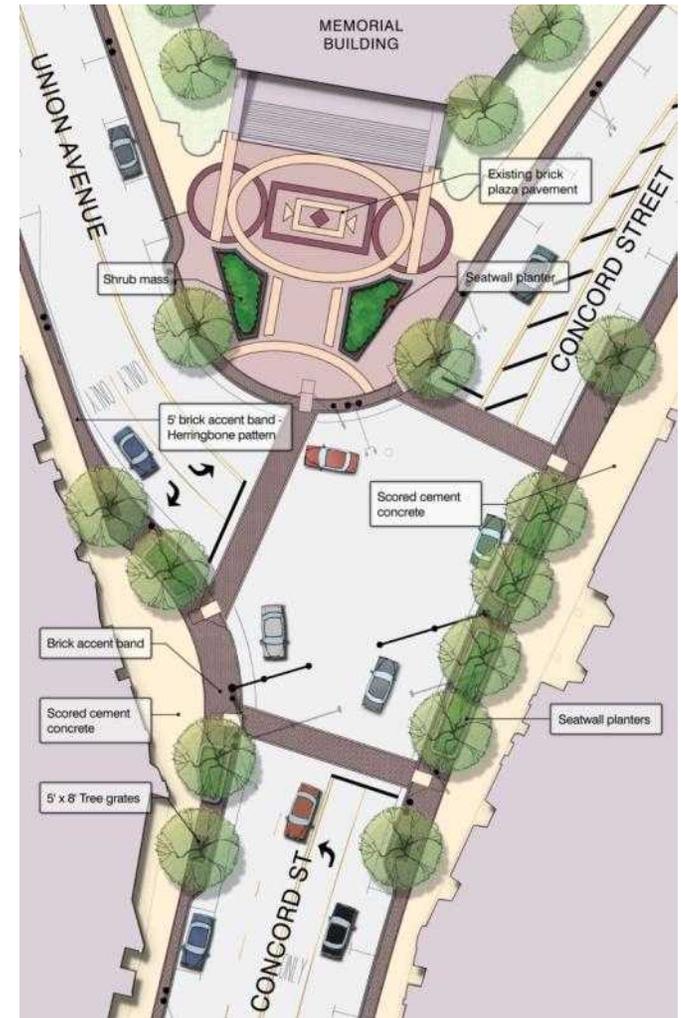


Credit: Framingham Patch



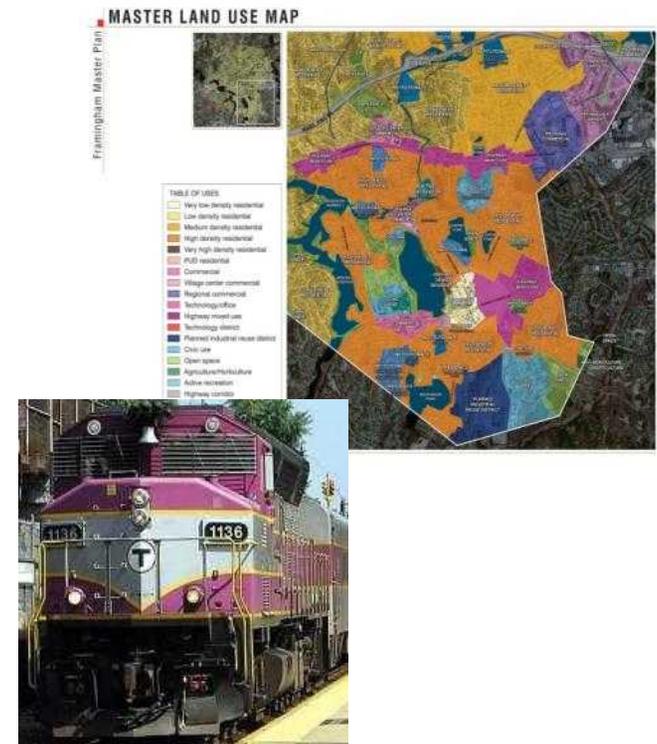
Recent Public Investment: Construction

- 2011 - Franklin Street Reconstruction
- 2012 - Sewer/Water Improvements (ARRA funding)
- Library
- 2014 (planned start) - Concord Street Roadway and Streetscape Improvements



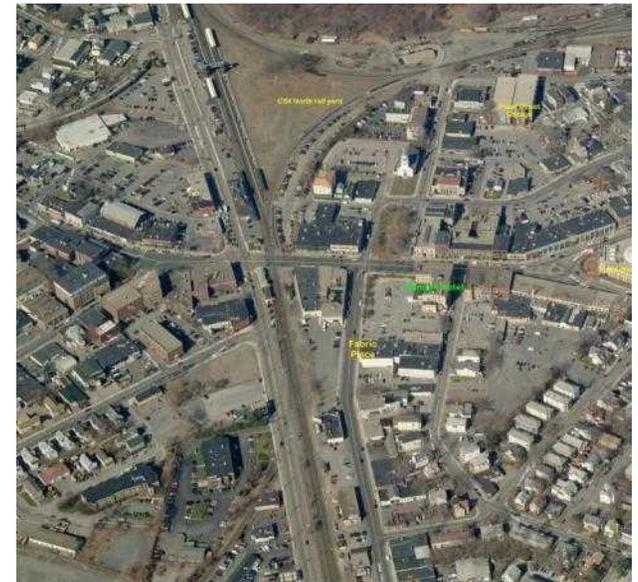
Recent Public Investments: Other

- Federal CDBG Grant Funding used to support Framingham Downtown Renaissance (FDR)
- 126/135 Grade Separation Studies
- Increased Service on Commuter Rail
- Mass Bay Community College



Opportunities

- CB Zoning in 1999 - Revisit
- Underdeveloped Sites/CSX
- Master Plan for Downtown (2009 and 2012)
- MAPC – Downtown Implementation Plan (2014)
- State Priority Development Area (495 Compact)
- Brownfields - AWP per consensus?



T.O.D. Focus Areas for Today's Discussion

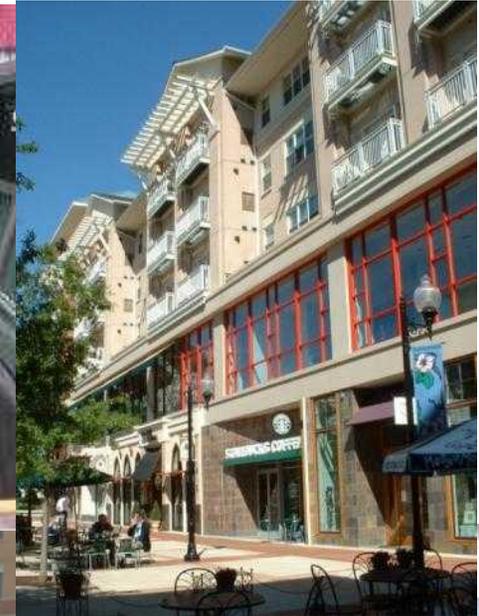
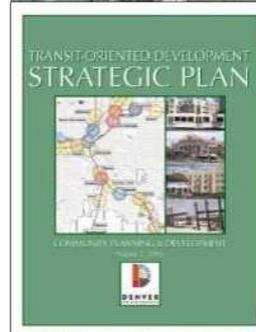
Sarah Lewis
Fuss & O'Neill

What is Transit-Oriented Development?

Mixed-use walkable development centered around transit

Characteristics:

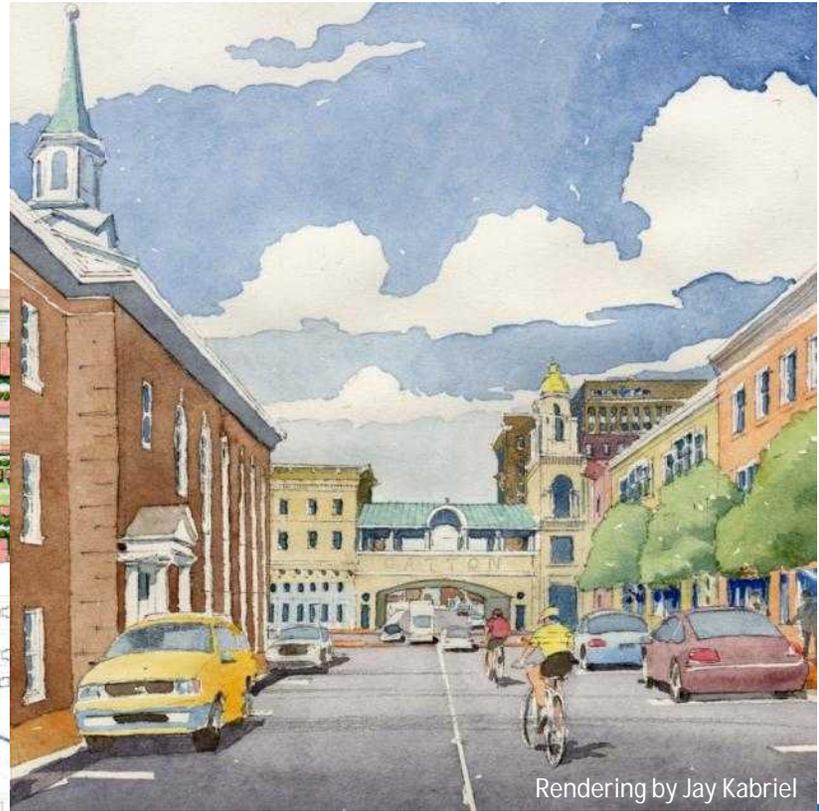
- High Quality Pedestrian and Civic Spaces
- Bike and Pedestrian Friendly
- Parking Management
- Variety of Housing Options
- Alternative Modes of Transportation
- 5 Minute walk for daily needs



Make it Better with a Vision

To succeed a vision should be:

- Oriented toward the Future but based in Reality
- Stakeholder Centered
- Collaborative and Educational
- Focused on Implementation
- Flexible (ULI)



Rendering by Jay Kabriel

Apply the Power of Partnerships

The public sector has the power to resolve land assembly issues, ensure the site is development-ready, ease the entitlement process, contribute land, and help fund infrastructure costs.

Private developers bring the real estate expertise, market connection to end users, and financial strategy and resources. (ULI)



Riverside Station Mixed-Use:
225,000 gsf office,
17,000 gsf of retail
290 unit residential condos
Totaling approx. 588,000 gsf with 1,028
spaces of parking
Plus an 8,000 gsf Community Center

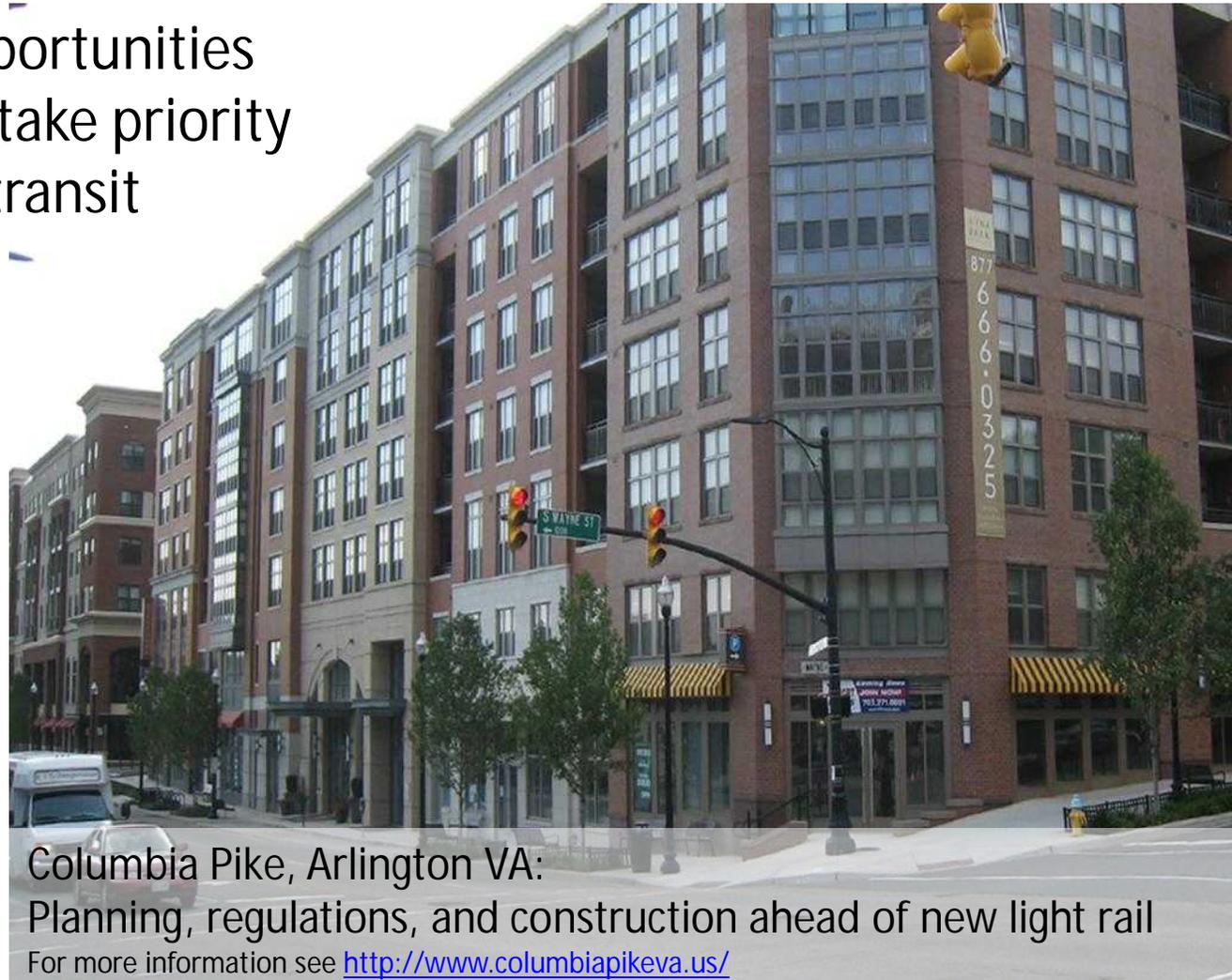
For more information see

<http://www.newtonma.gov/gov/aldermen/committees/landuse/riverside.asp>

Think Development when thinking Transit

Real estate opportunities should always take priority over low-cost transit solutions.

(eg. Rosslyn-Ballston Corridor: Running transit along the median of an interstate may save right-of-way costs but preclude development options) (ULI)



Get the Parking Right

Too much parking makes the area less pedestrian friendly and wastes space that could be used for the types of development that increase ridership.

Move It: The land nearest the station is the best land for development, so using it for parking means a lost opportunity. (Think parking - 5 to 7 min. walk to station.)

Share It: Share parking among patrons who make use of it at different times. Shared parking can be operated privately or by a local parking authority and fees can offer facility financing.

Wrap It: Wrapped retail, service shops, restaurants, and/or residences adds to a more seamless sense of place experience.

(ULI)

Build a Place, Not a Project

Transit-centered communities require attention to the scale and design of surrounding developments.

It is Essential to engage all principals (the transit agency, the local government, the citizens, and the participating developers). (ULI)



Pleasant Hill BART Station
Contra Costa Transit Village, CA
For more information see <http://www.contracostacentre.com/>



Market-Driven Retail, not Transit Driven

Public agencies must resist requiring retail as part of a TOD without an adequate market demand (analysis). Better to have a few busy, successful stores than many empty ones. (ULI)

Uptown District, Portsmouth VA:
Transit-Ready Infill and Revitalization

For more information see <http://www.portsmouthva.gov/planning/uptownguidelines.aspx>



Mix Uses

A Mix of Uses along a Corridor will render the corridor itself attractive as a Community. (ie. Rosslyn-Business; Clarendon-Urban Village, C Line Light-rail in Denver, etc.) (ULI)



MBTA Wonderland Station
Phase I:
8-story office building,
125 room hotel
149 unit residential condos
17,000 gsf of retail
240 spaces of parking totaling
approx. 430,000 gsf
Public improvements will
include a public plaza with
pedestrian bridges
connecting to Revere Beach.
For more information, contact Phil Jean at
pjean@transitrealty.com

Think about Station to Destination

Plan for more than just a site. Pedestrians are of value on foot and their experience between anchors is important. (ULI)



Windsor Locks Amtrak
Station Relocation Transit-
Oriented Development Study
32,900 gsf office
47,000 gsf retail/restaurant
142 for sale residential
162 rental residential

For more information, contact Patrick
McMahon at wleidc@sbcglobal.net or
Sarah Lewis at salewis@fando.com

Every Price Point Living around Transit

Have enough affordable units to not price out all of what makes the Sense of Place

People from every part of the economic spectrum like to live near transit

Developments can make over 15% profit on affordable housing

- Professor Howard Bell (Columbia University Real Estate Program) (ULI)



Engage Corporate Attention

Some companies had to move because they found it difficult to recruit scarce talent because of commute times. (ULI)



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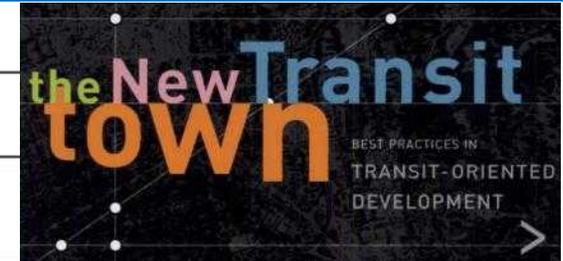


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Best Practices

TABLE 1.1 — PROFILE OF CASE STUDY PROJECTS

PROJECT	DEVELOPER	DATE COMPLETED	LAND USES	TRANSIT	FINANCING		
Arlington County Virginia	County plans Various private	As of 2000	17.9M sq.ft. office 3.0M sq.ft. retail 21,581 housing units	Heavy-rail Bus	Public/Private		
Mockingbird Station Dallas, TX	Ken Hughes	2000	214,000 sq.ft. residential 183,000 sq.ft. retail	Light-rail Bus	Private	1.0/bedroom 3.23/1,000 gross sq.ft. retail	24 units/acre
Addison Circle Addison, TX	Columbus Realty Trust Post Properties	Phase 3 in 2002	1,800 apts. 86 condos 6 town homes 115,000 sq.ft. retail 342,000 sq.ft. office	Bus Light-rail planned	Public/Private	Phase 1: 1/bedroom Phase 2: 0.3/bedroom Phase 3: 1/bedroom 3.7 spaces/1,000 gross sq.ft. retail 3.2 spaces/1,000 gross sq.ft. office	100 units/acre
Mercado San Diego, CA	MAAC Landgrant Richard Juarez	Apartments in 1993	138,000 sq.ft. residential 144 apts. 118,000 sq.ft. retail	Light-rail Bus	Public/Private Affordable LIHTC	1.5 /unit 3.5/1,000 gross sq.ft. retail	32.7 units/acre
Lindbergh Atlanta, GA	Carter & Assoc.	Phase 1 in 2003	388,000 sq.ft. residential 1M sq.ft. office 330,000 sq.ft. retail	Heavy-rail Bus	Public/Private	2.2/1,000 sq.ft. commercial less than 1 per bedroom	
Ohlone Court Santa Clara County	Bridge Housing	1997	135 units	Light-rail Bus	Public/Private Affordable	1.5/1.8/2.0 spaces for 1/2/3 bedrooms	22.1 units/acre
Ohlone-Chynoweth Commons Santa Clara County	Eden Housing	2001	194 units	Light-rail Bus	Public/Private Affordable	same	26.6 units/acre
1 Pearl Avenue Santa Clara County	Clker Orchards	2003	182 units	Light-rail Bus	Private	same	41.4 units/acre

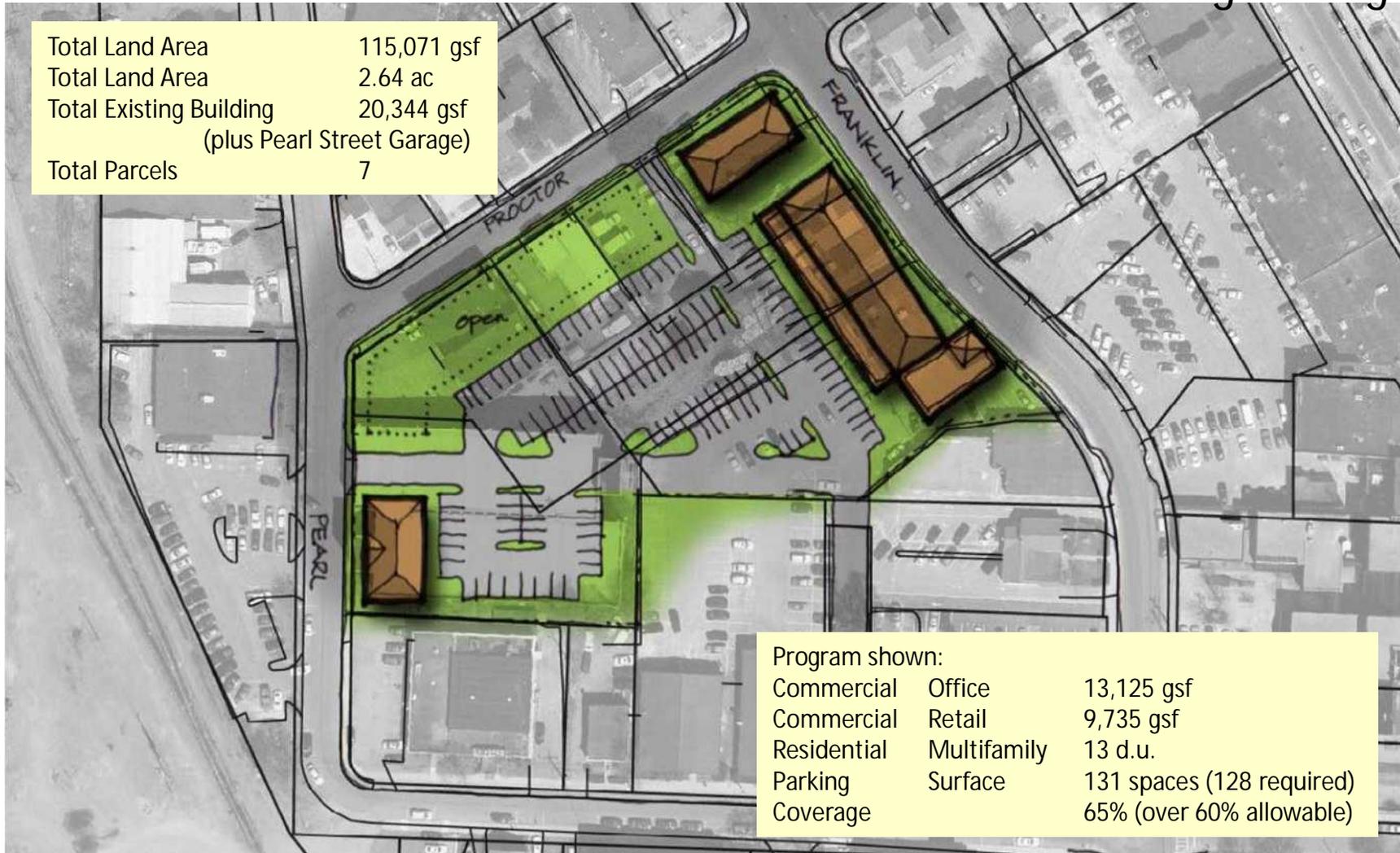


Possible Assemblage Sites



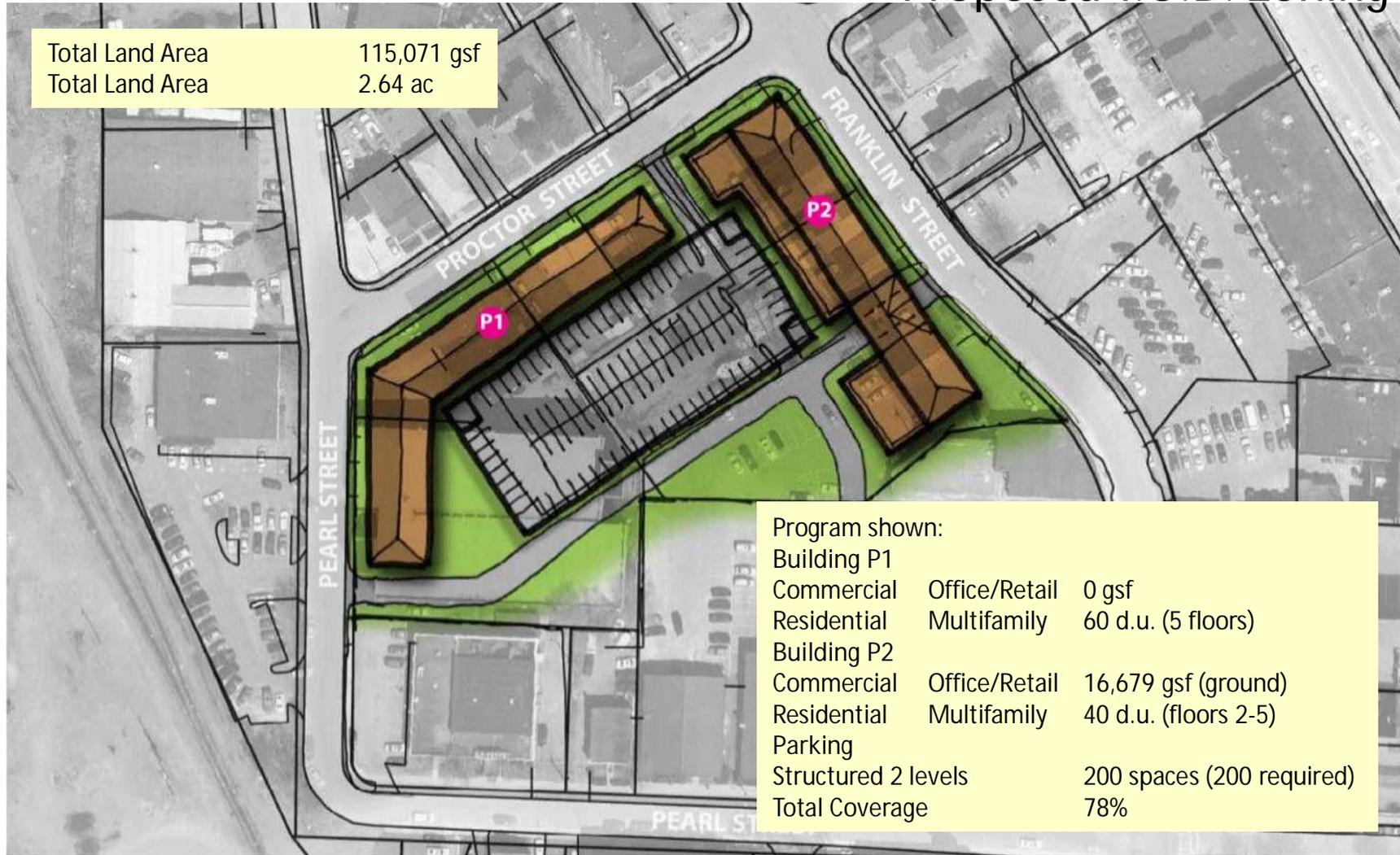
Pearl Street Assemblage

Existing Zoning



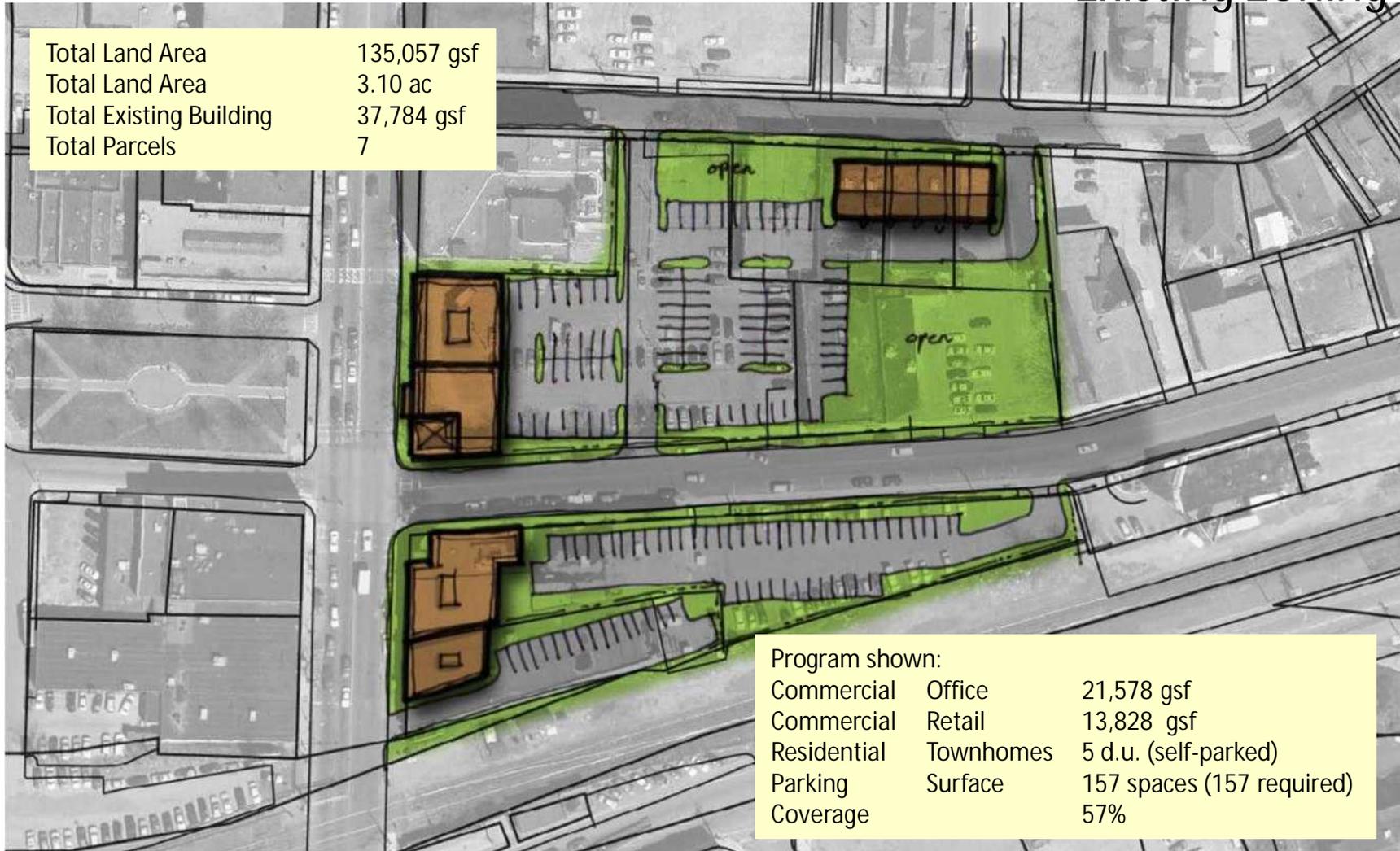
Pearl Street Assemblage

Proposed T.O.D. Zoning



Howard Street Assemblage

Existing Zoning



Howard Street Assemblage

Proposed T.O.D. Zoning



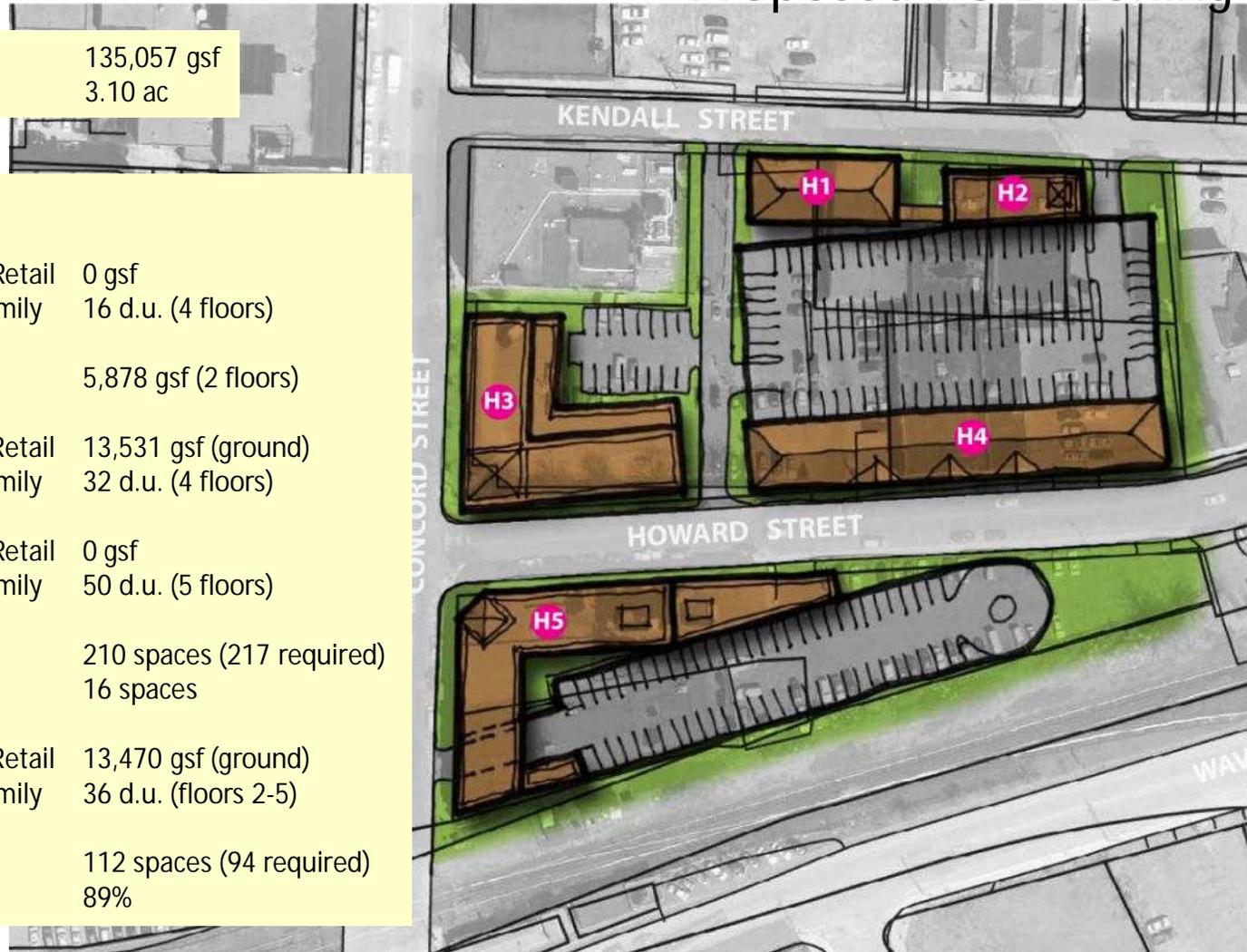
Howard Street Assemblage

Proposed T.O.D. Zoning

Total Land Area 135,057 gsf
 Total Land Area 3.10 ac

Program shown:

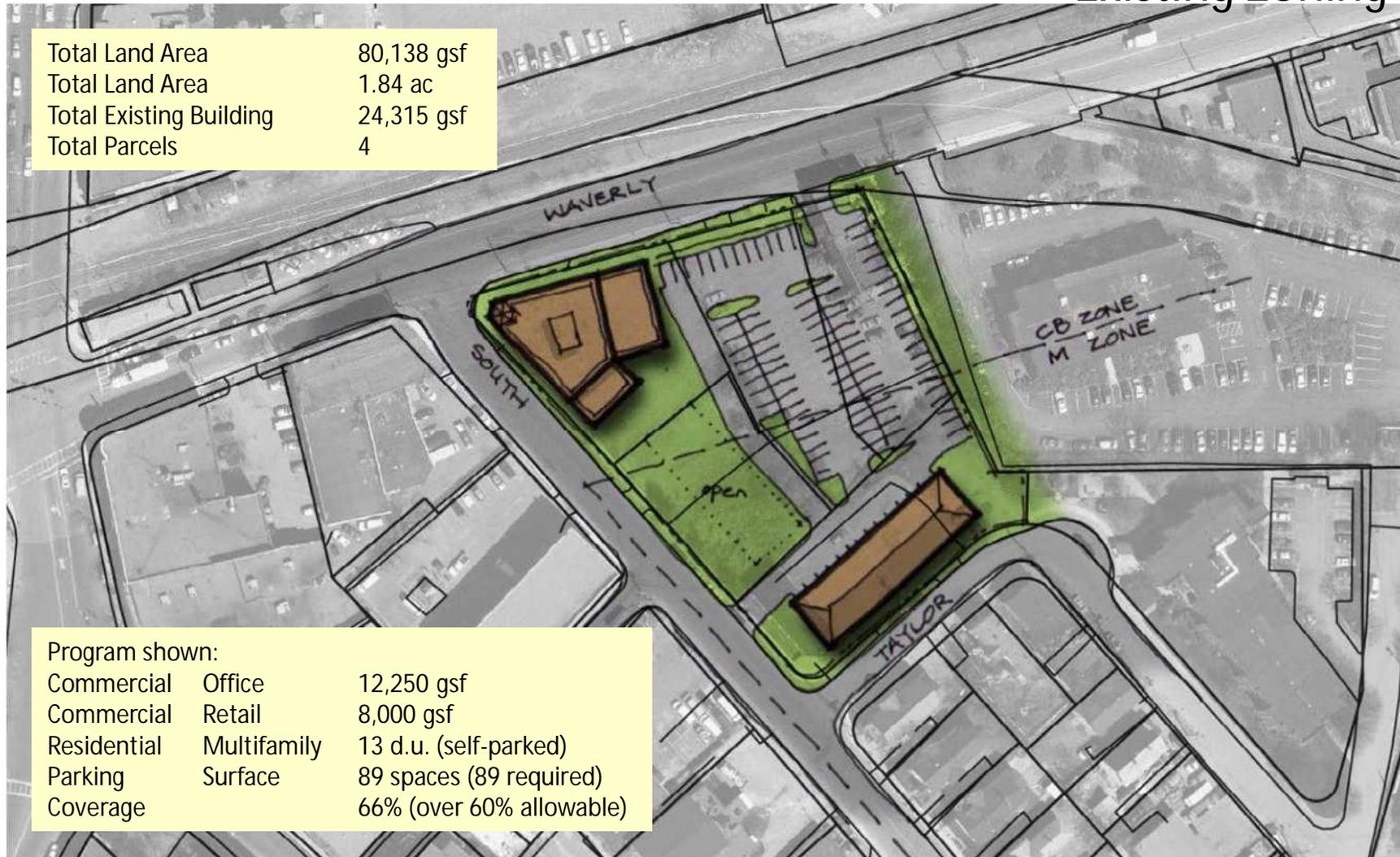
Building H1		
Commercial	Office/Retail	0 gsf
Residential	Multifamily	16 d.u. (4 floors)
Building H2		
Commercial	Church	5,878 gsf (2 floors)
Building H3		
Commercial	Office/Retail	13,531 gsf (ground)
Residential	Multifamily	32 d.u. (4 floors)
Building H4		
Commercial	Office/Retail	0 gsf
Residential	Multifamily	50 d.u. (5 floors)
Parking (north)		
Structured 2 levels		210 spaces (217 required)
Surface		16 spaces
Building H5		
Commercial	Office/Retail	13,470 gsf (ground)
Residential	Multifamily	36 d.u. (floors 2-5)
Parking (south)		
Structured 2 levels		112 spaces (94 required)
Total Coverage		89%



South Street Assemblage

Existing Zoning

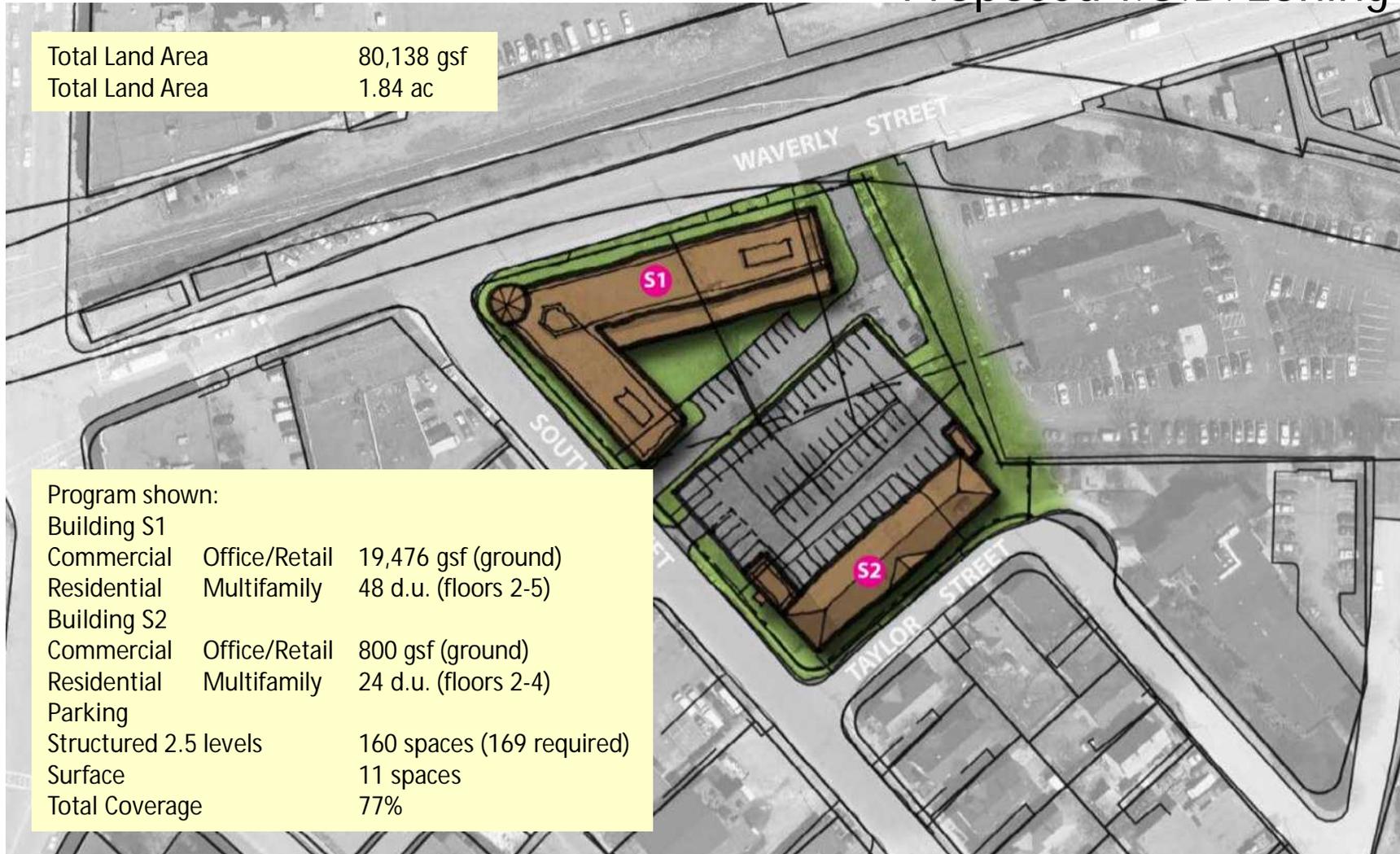
Total Land Area	80,138 gsf
Total Land Area	1.84 ac
Total Existing Building	24,315 gsf
Total Parcels	4



Program shown:		
Commercial	Office	12,250 gsf
Commercial	Retail	8,000 gsf
Residential	Multifamily	13 d.u. (self-parked)
Parking	Surface	89 spaces (89 required)
Coverage		66% (over 60% allowable)

South Street Assemblage

Proposed T.O.D. Zoning



Hollis Court Assemblage

Existing Zoning



Hollis Court Assemblage

Proposed T.O.D. Zoning



Hollis Court Assemblage

Proposed T.O.D. Zoning

Total Land Area 252,718 gsf
 Total Land Area 5.08 ac

Program shown:

Building HC1			
Commercial	Office/Retail	19,271 gsf (ground)	
Residential	Multifamily	52 d.u. (floors 2-5)	
Building HC2			
Commercial	Office/Retail	14,050 gsf (ground)	
Residential	Multifamily	44 d.u. (floors 2-4)	
Building HC4			
Residential	Multifamily	25 d.u. (5 floors)	
Building HC3			
Residential	Multifamily	50 d.u. (5 floors)	
Building HC5			
Commercial	Office/Retail	13,200 gsf (ground)	
Residential	Multifamily	18 d.u. (floors 2-4)	
Parking			
Structured 7 levels		196 spaces	
Surface		24 spaces	
Structured 2.5 levels		210 spaces	
Surface		73 spaces	
Structured 3 levels		120 spaces	
Total Provided	623 spaces including 72 transit spaces		
Total Coverage		77%	



Recommendations

- Consider district-wide requirements instead of per parcel basis
- Change parking ratios to 3 spaces per 1,000 gsf for all commercial uses (Retail, Restaurant, and Office)
- Change parking ratios to 1.5 space per unit for all Residential types
- Increase coverage permitted to 80% maximum
- Streamline approval process

Panel Discussion

Perspectives: Development Community

- Roy S. MacDowell, Partner, Baystone Development Co.
- Ed Zuker, Principal, Chestnut Hill Realty
- Justin Krebs, Normandy Realty Partners

Questions & Discussion

Conclusions & Next Steps

Robert Halpin and
Sarah Lewis

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Brownfields

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